



# मेरठ विकास प्राधिकरण

अक्रमेणानुपायेन कर्मरम्भो न सिध्यति



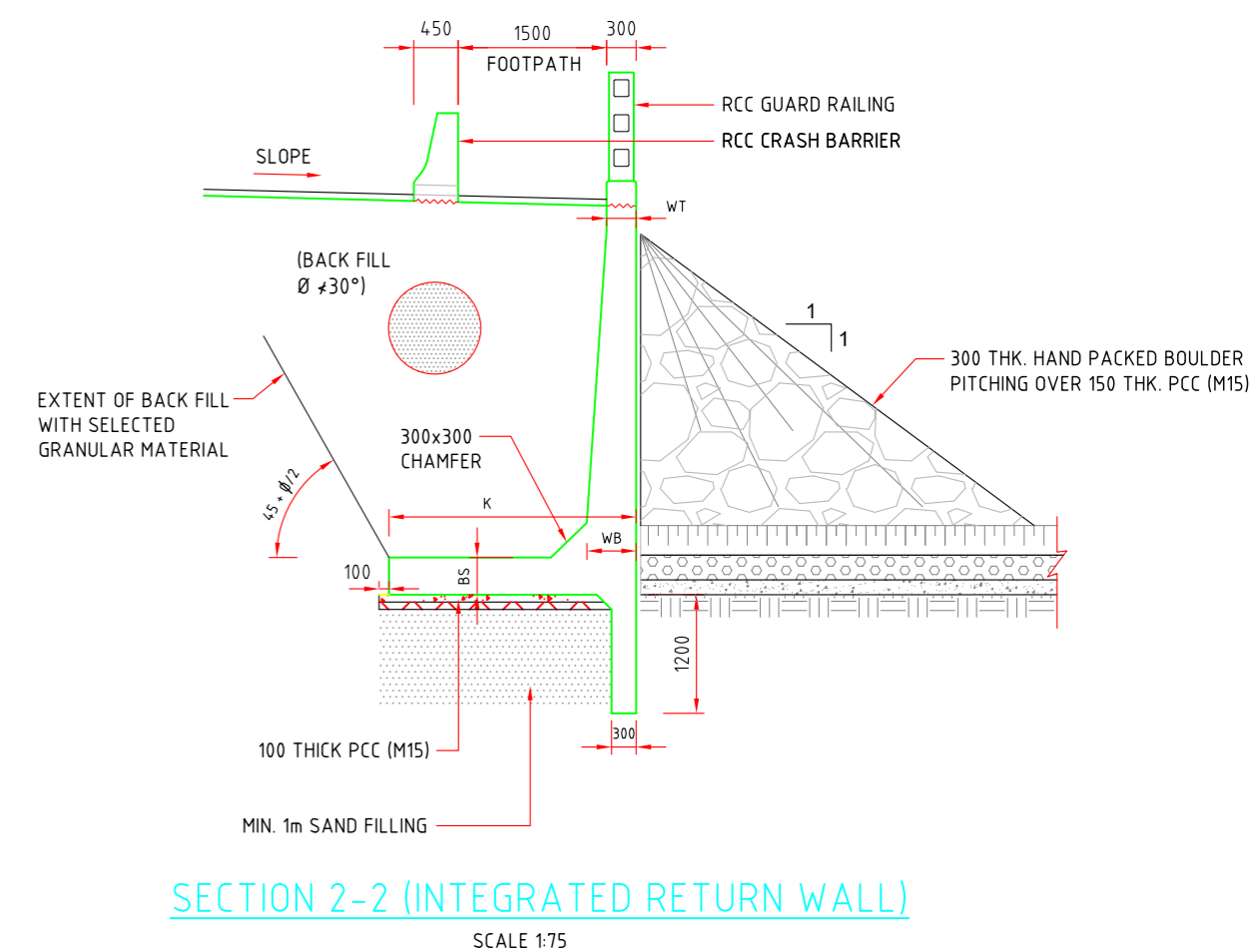
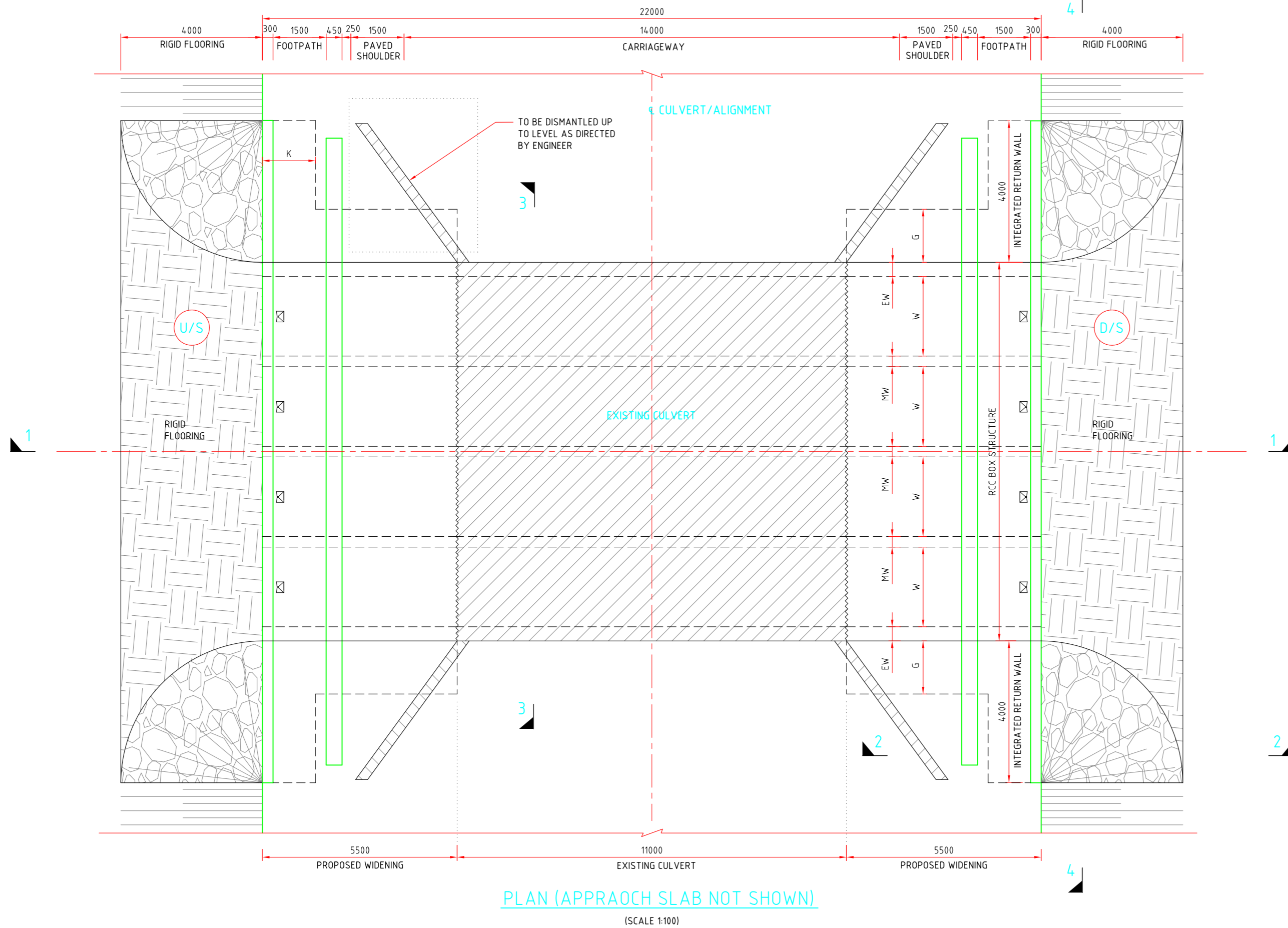
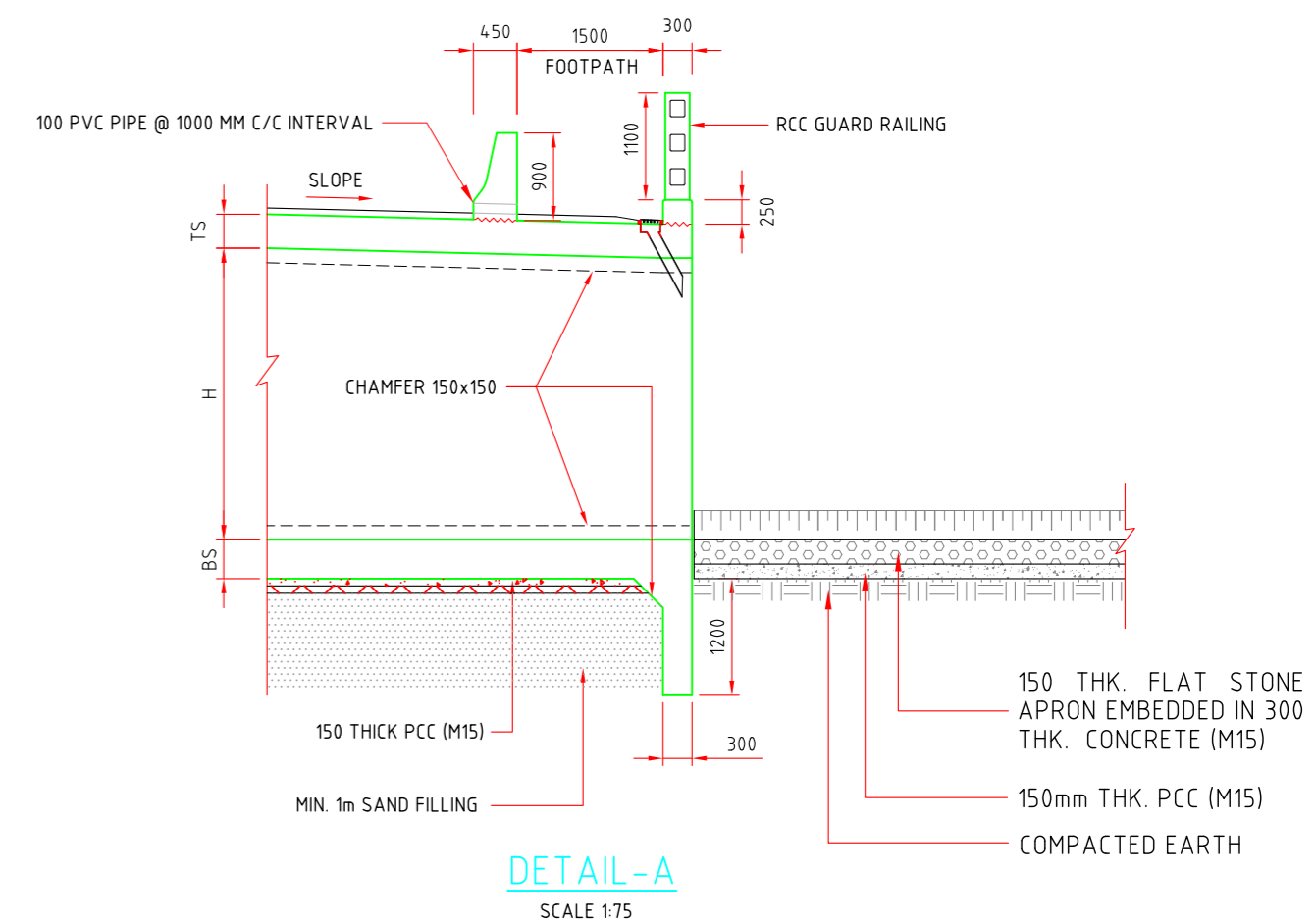
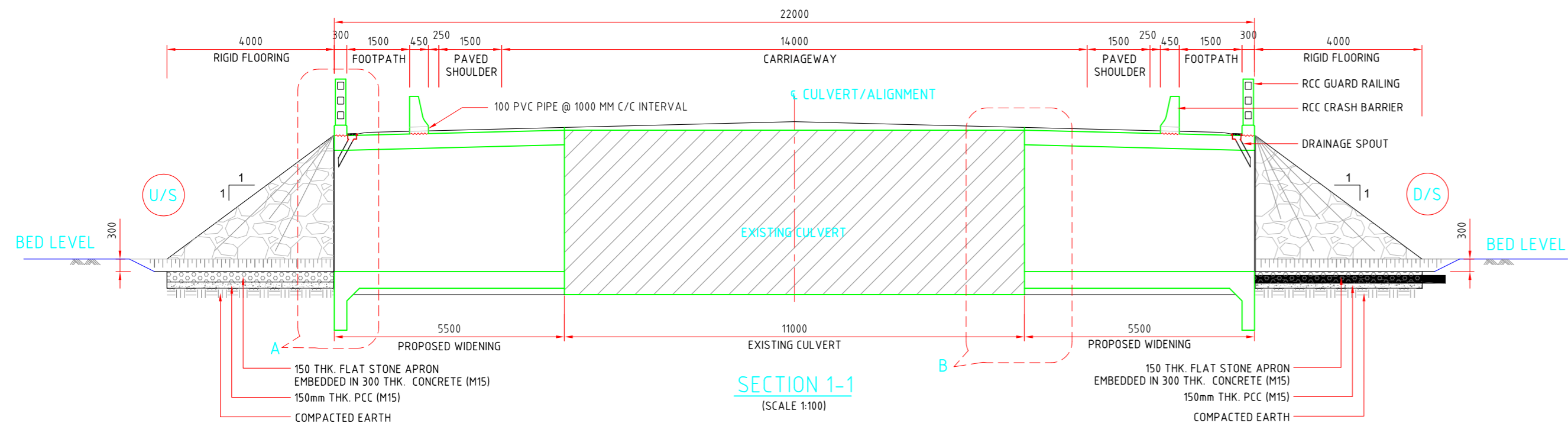
पत्रांक :- MeDA/24-25/EN/7771-2

दिनांक :- 22/02/2025

## शुद्धि पत्र

कार्य का नाम- ईस्टर्न कचहरी रोड, मवाना रोड व मंगल पाण्डेनगर से लोकप्रिय हास्पिटल रोड के आबू नाले पर स्थित पुलिया के चौड़ीकरण का कार्य।

मेरठ विकास प्राधिकरण द्वारा अवस्थापना निधि के अन्तर्गत ईस्टर्न कचहरी रोड, मवाना रोड व मंगल पाण्डेनगर से लोकप्रिय हास्पिटल रोड के आबू नाले पर स्थित पुलिया के चौड़ीकरण का कार्य की ई-निविदा मुख्य अभियन्ता के कार्यालय द्वारा जारी पत्रांक MeDA/24-25/EN/7771-1 दिनांक 07/02/2025 के द्वारा आमंत्रित की गयी थी। जिसमें निविदा प्रपत्रों के साथ त्रुटिवश ले-आउट प्लान की ड्राइंग संलग्न नहीं की गयी थी। उक्त ले-आउट प्लान की ड्राइंग को संलग्न करने का कष्ट करें। इसके अतिरिक्त नियम व शर्तों में कोई परिवर्तन नहीं किया जायेगा।



### NOTES :

- ALL DIMENSIONS ARE IN MILLIMETERS, LEVELS AND CHAINAGES ARE IN METERS UNLESS OTHERWISE STATED.
- DIMENSIONS ARE NOT TO BE SCALED ONLY WRITTEN DIMENSIONS ARE TO BE FOLLOWED.
- THIS DRAWING IS READ IN CONJUNCTION WITH SHEET 2 OF 2 OF THIS DRAWING.

CLIENT : -  
Meerut Development Authority  
(MDLA)

DPR CONSULTANT :-  
RITES LIMITED



PROJECT :-

Consultancy Services for Preparation of Detailed  
Project Report (DPR) and Construction Supervision  
of all Infrastructure works

DRAWING STATUS :-

R-0	JAN 25	Tender Drawings
REV	DATE	DESCRIPTION

APPROVED BY

CHECKED BY

DESIGNED BY

DRAWN BY

TITLE

DRAWING NO.

SCALE

Widening of Minor Bridge on Meerut - Mawana Road  
(NH-34) near Defence Colony

AS ABOVE

SHEET:- 01 of 02

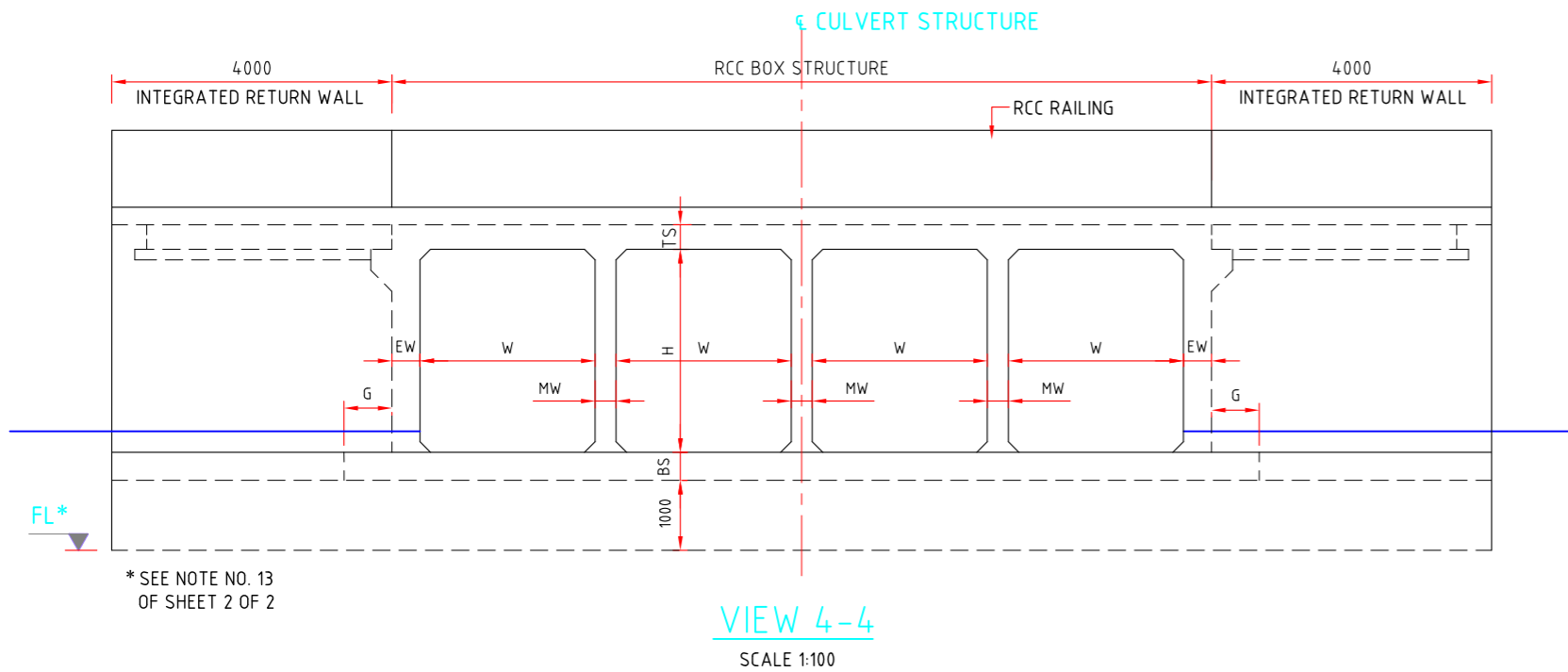
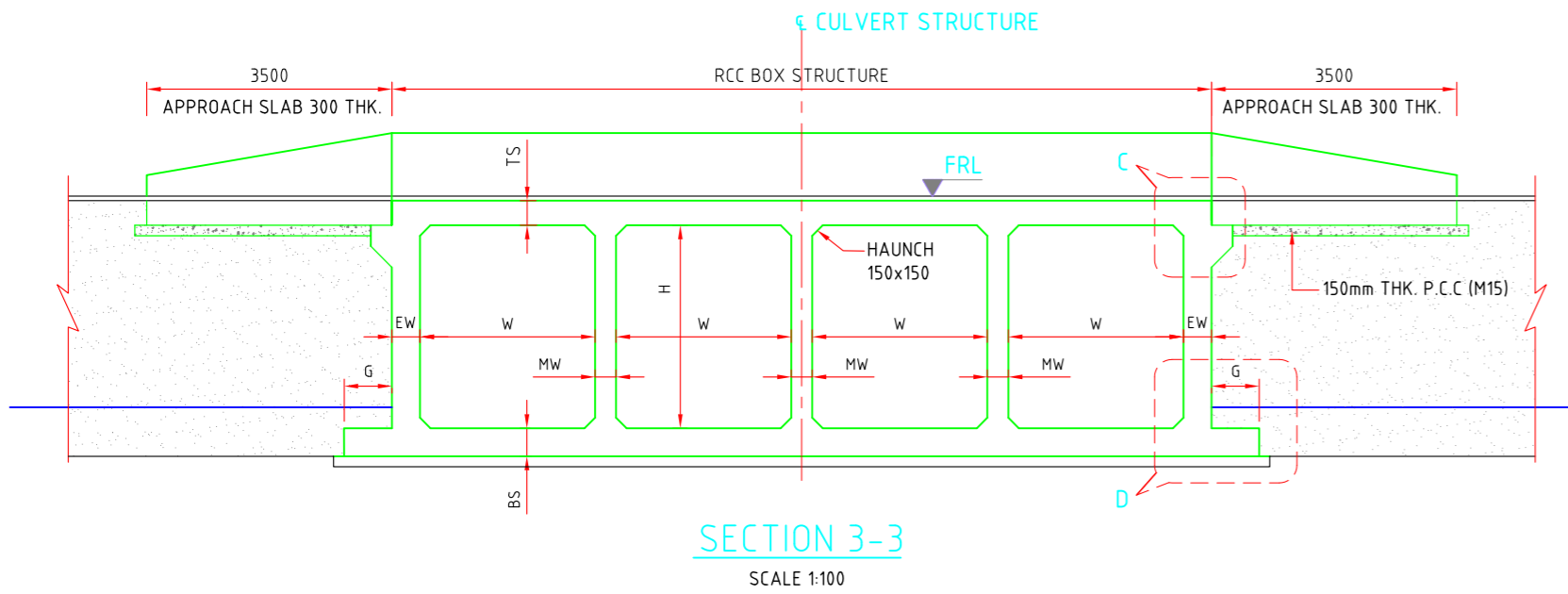
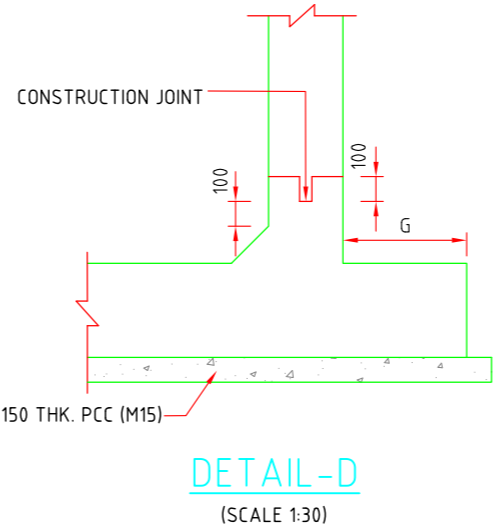
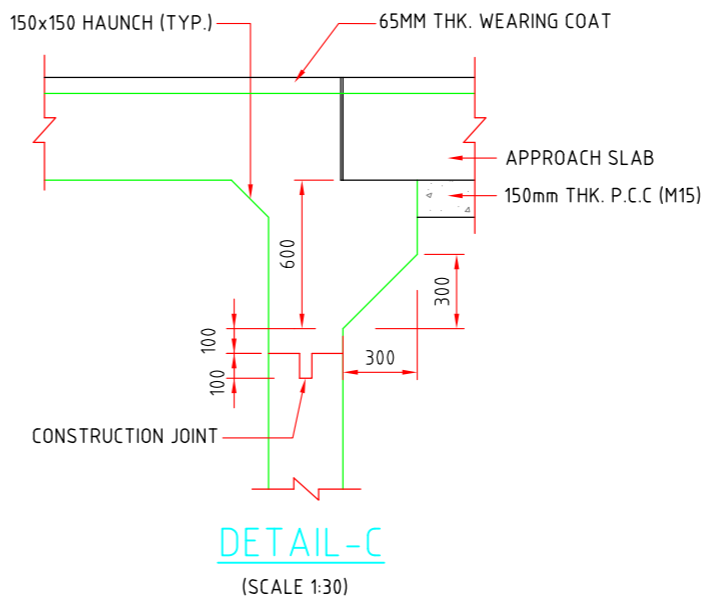
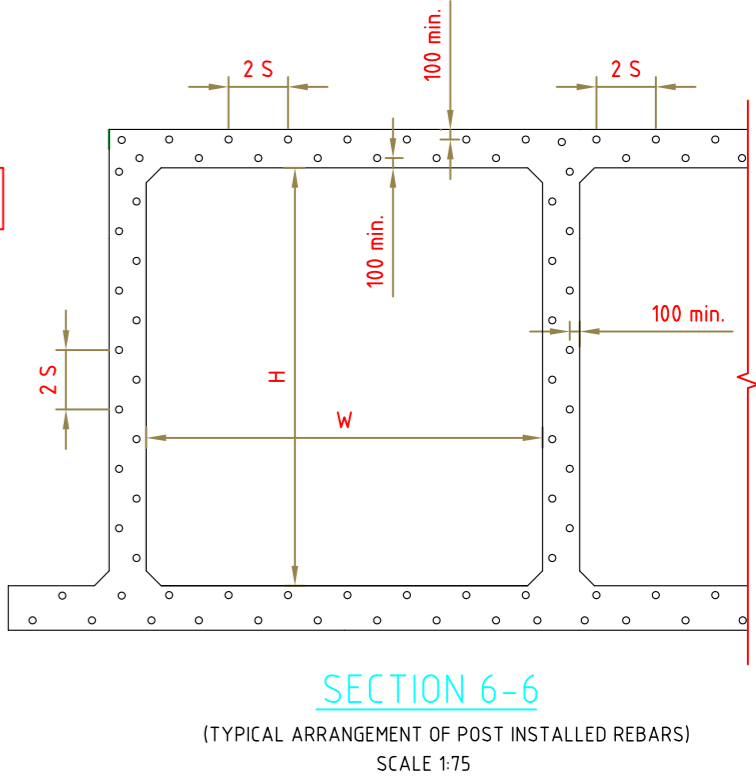
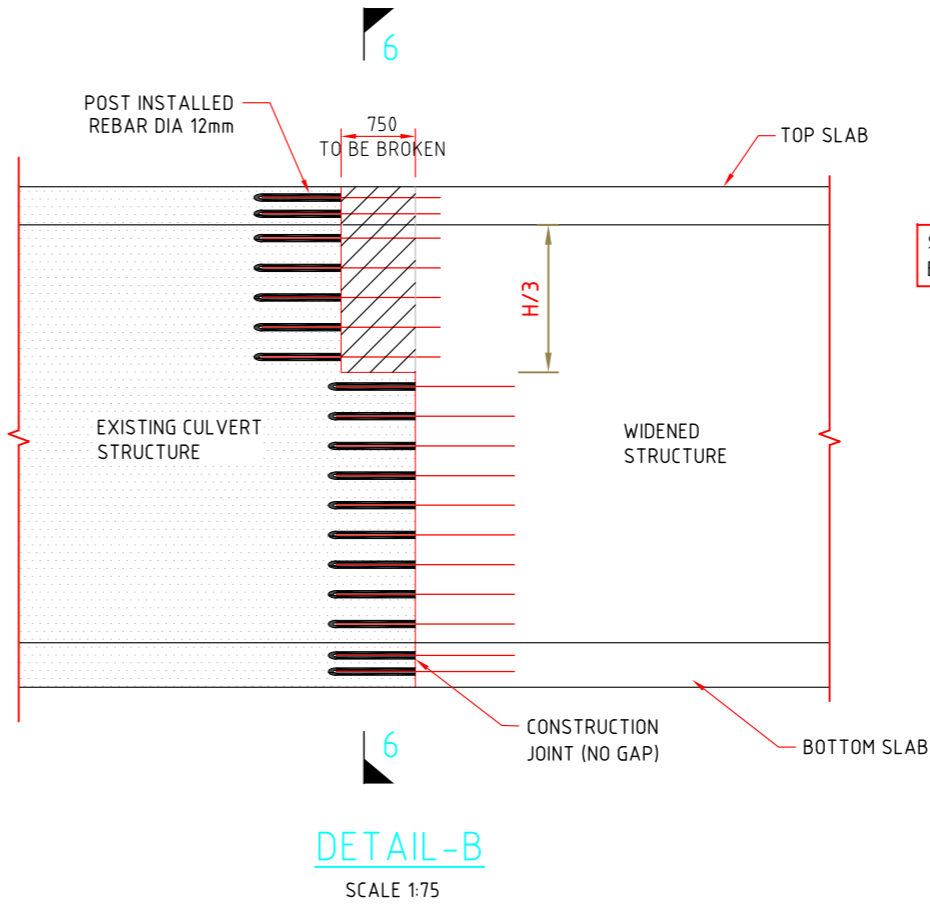


TABLE 1: DIMENSIONS FOR 4-VENT CULVERT

CULVERT SIZE	W	H	EW	MW	TS	BS	G	K	WT	WB
4x3000x4000	3000	4000	350	Refer note 12	350	400	500	1700	300	500

\* FOR DESIGN PURPOSE, MAXIMUM "H" HAS BEEN CONSIDERED AS 4000MM. "H" SHALL BE VERIFIED AT SITE BY ENGINEER-IN-CHARGE AND "H" ABOVE 4000MM SHALL BE REPORTED TO THE DESIGN CONSULTANT.



NOTES:

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- DIMENSIONS ARE NOT TO BE SCALED, ONLY WRITTEN DIMENSIONS ARE TO BE FOLLOWED.
- ALL LEVELS AND CAMBER/SUPER-ELEVATION SHALL BE MAINTAINED WITH ROAD PROFILE AT THE BRIDGE LOCATION AND SHALL BE VERIFIED BY ENGINEER IN CHARGE AT THE TIME OF CONSTRUCTION.
- DURING CONSTRUCTION OF THE BRIDGE EXISTING ROAD TRAFFIC SHALL BE DIVERTED SUITABLY IN CONSULTATION WITH ROAD AUTHORITY.
- CONCRETE SHALL BE DESIGN MIX WITH MINIMUM CHARACTERISTIC STRENGTH FOR VARIOUS STRUCTURES SHALL BE AS FOLLOWS
  - BOX SECTION M30
  - RETURN / RETAINING WALL / APPROACH SLAB M30
  - CRASH BARRIER M40
- THE REINFORCEMENT SHALL BE OF THERMO MECHANICALLY TREATED (HYSD) BARS (GRADE DESIGNATION Fe500D CONFORMING TO IS 1786-2008.
- INVERT LEVEL SHALL NOT BE LESS THAN 300 MM BELOW THE EXISTING BED LEVEL. HOWEVER THE INVERT LEVELS CAN BE MODIFIED AS PER SITE CONDITION IN CONSULTATION WITH ENGINEER-IN-CHARGE.
- FOR DESIGN PURPOSE, SAFE BEARING CAPACITY (S.B.C) HAS BEEN CONSIDERED AS 15.0 TON/SQM WHICH SHALL BE VERIFIED BEFORE CONSTRUCTION.
- SOFT AND LOOSE PATCHES IN THE BEARING AREA SHALL BE REPLACED BY COMPACTED GRANULAR FILL WITH LAYERS NOT EXCEEDING 300mm.
- THE ANGLE OF REPOSE ( F ) OF BACKFILL MATERIAL BEHIND RCC BOX, RETURN WALL, RETAINING WALL SHALL NOT BE LESS THAN 30°. BACKFILL MATERIAL SHALL BE FILLED AT AN ANGLE (45 + F / 2) WITH HORIZONTAL FROM BASE OF THE FOOTING.
- SURFACE OF EXISTING CONCRETE MEMBER SHALL BE THOROUGHLY CLEANED AND ROUGHENED UP BY CHIPPING. CEMENT SLURRY SHALL BE APPLIED ON THE ROUGHENED SURFACE BEFORE POURING FRESH CONCRETE ON IT.
- INNER SURFACE OF THE EXISTING AND WIDENED MEMBER SHOULD BE FLUSHED AND ANY OFFSET BETWEEN THE SLAB/WALL THICKNESS OF EXISTING AND WIDENED MEMBER SHALL BE ON THE OUTER SURFACE.
- POST INSTALLED REINFORCING BAR SHALL BE USED TO DEVELOP MONOLITHIC CONNECTION BETWEEN EXISTING AND WIDENED MEMBER. THE POST INSTALLED REINFORCING BAR SHALL BE INSTALLED AS PER SPECIFICATION OF THE MANUFACTURER OF THE BAR.
- THE MINIMUM DEPTH OF FOUNDATION OF CANTILEVER RETAINING WALL BELOW SCOUR / PROTECTED BED LEVEL SHALL BE 1500mm.
- WEARING COAT SHALL COMPRISE OF 40mm BITUMINOUS CONCRETE OVERLOAD WITH 25mm THICK BITUMEN MASTIC LAYER.
- THE PROTECTION WORK SCHEME & EXTEND OF CRASH BARRIER SHOWN IN THE DRAWING IS TENTATIVE AND MAY UNDERGO CHANGES AT THE TIME OF DETAIL DESIGN AND CONSTRUCTION.

CLIENT :-  
Meerut Development Authority  
(MDLA)

DPR CONSULTANT :-  
RITES LIMITED



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Project Report (DPR) and Construction Supervision  
of all Infrastructure works

DRAWING STATUS :-

R-0	JAN 25	Tender Drawings
REV	DATE	DESCRIPTION

APPROVED BY

CHECKED BY

DESIGNED BY

DRAWN BY

TITLE

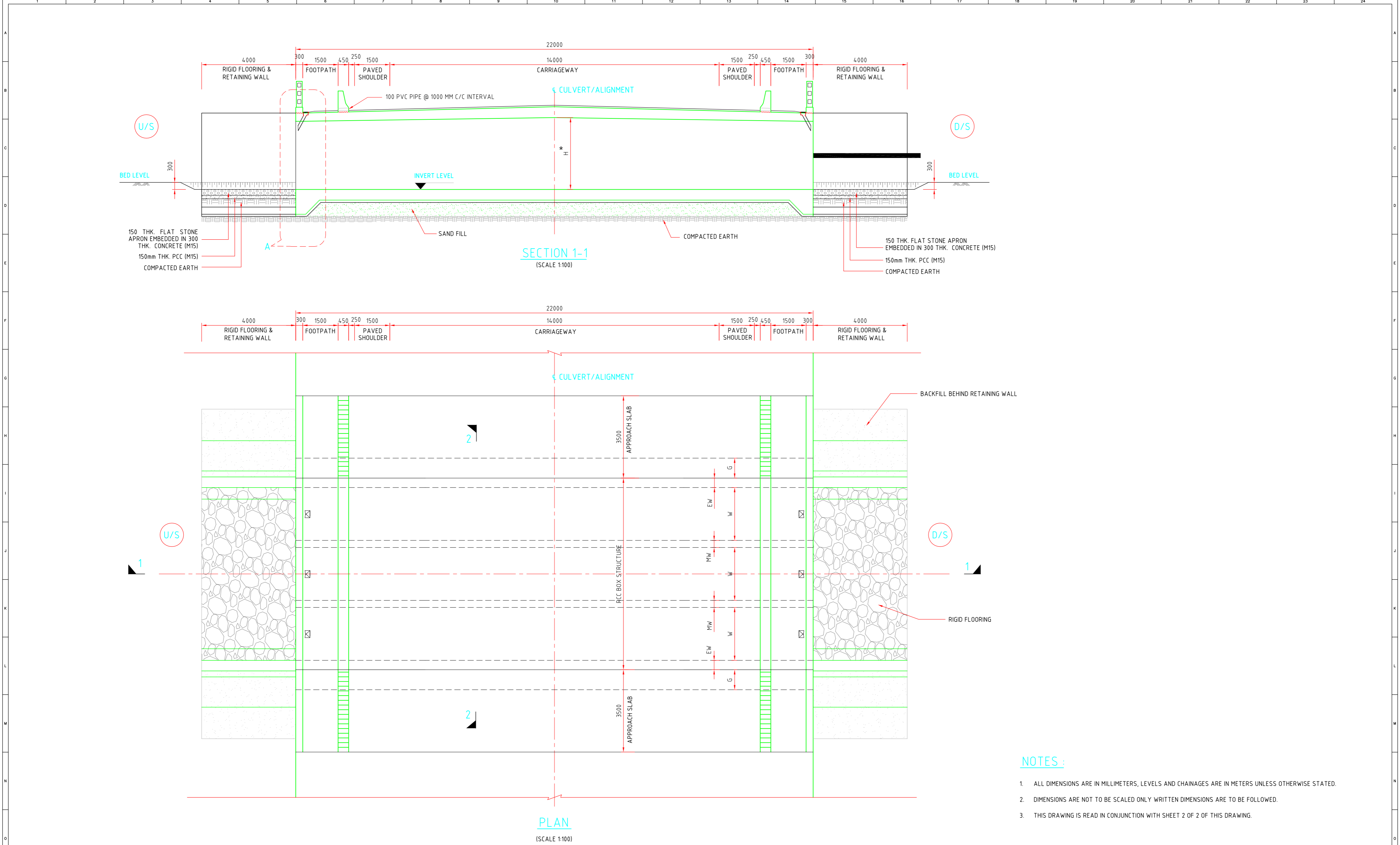
DRAWING NO.

SCALE

Widening of Minor Bridge on Meerut - Mawana Road  
(NH-34) near Defence Colony

AS ABOVE

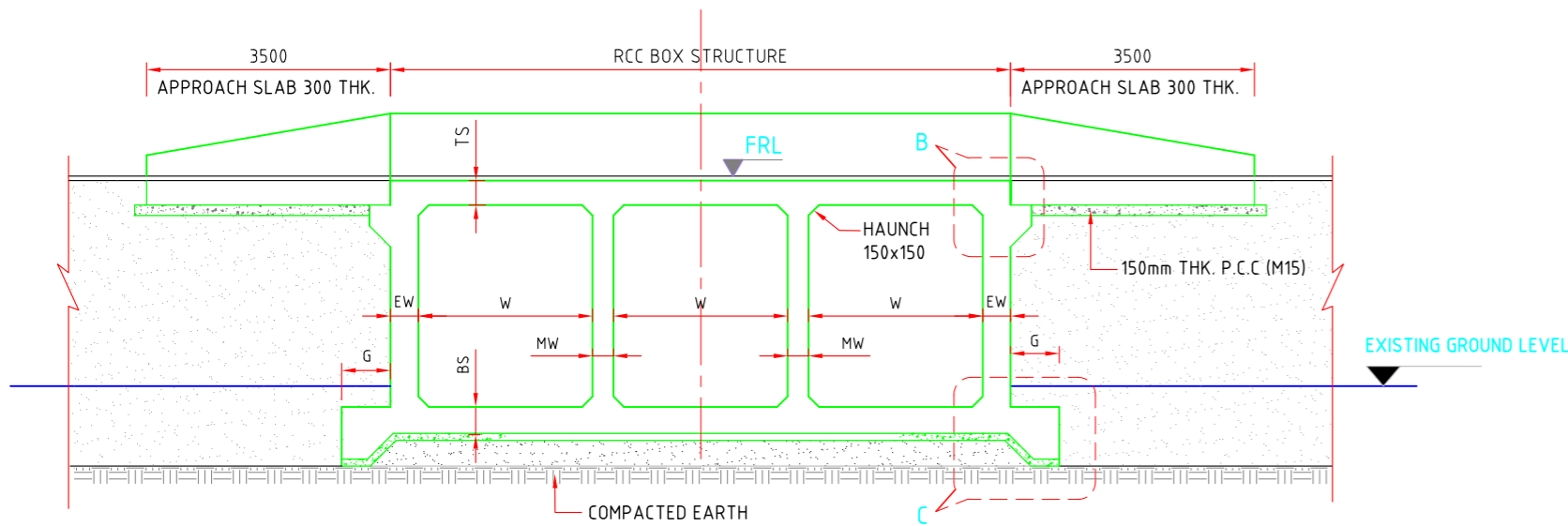
SHEET:- 02 of 02



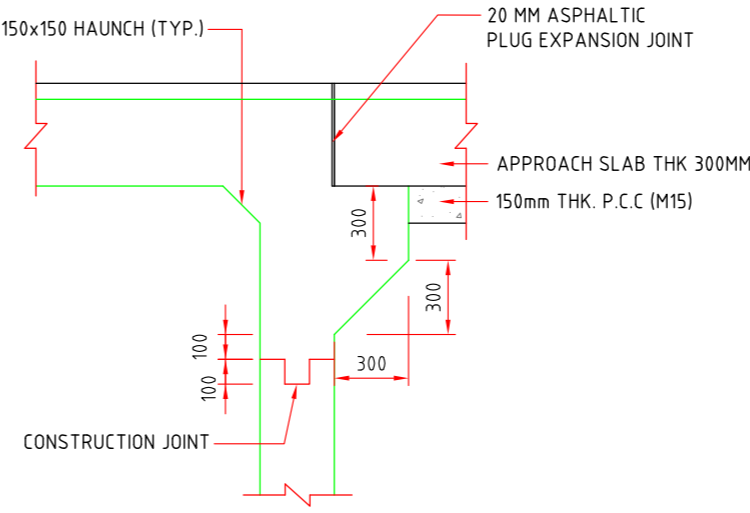
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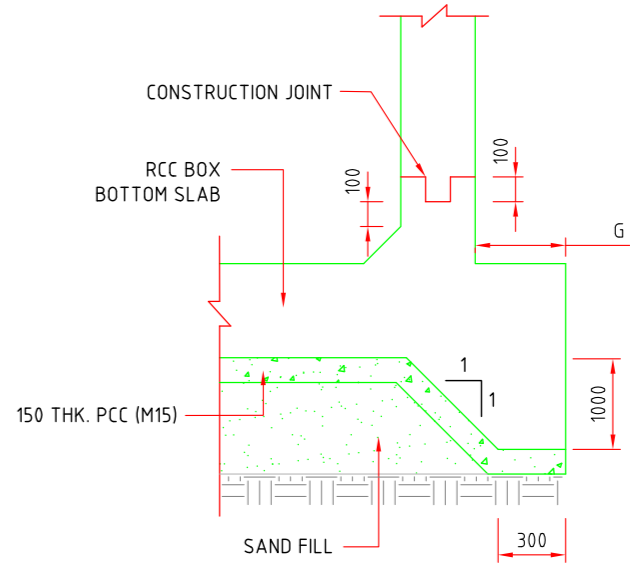
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SECTION 2-2  
SCALE 1:100



DETAIL-B  
(SCALE 1:30)

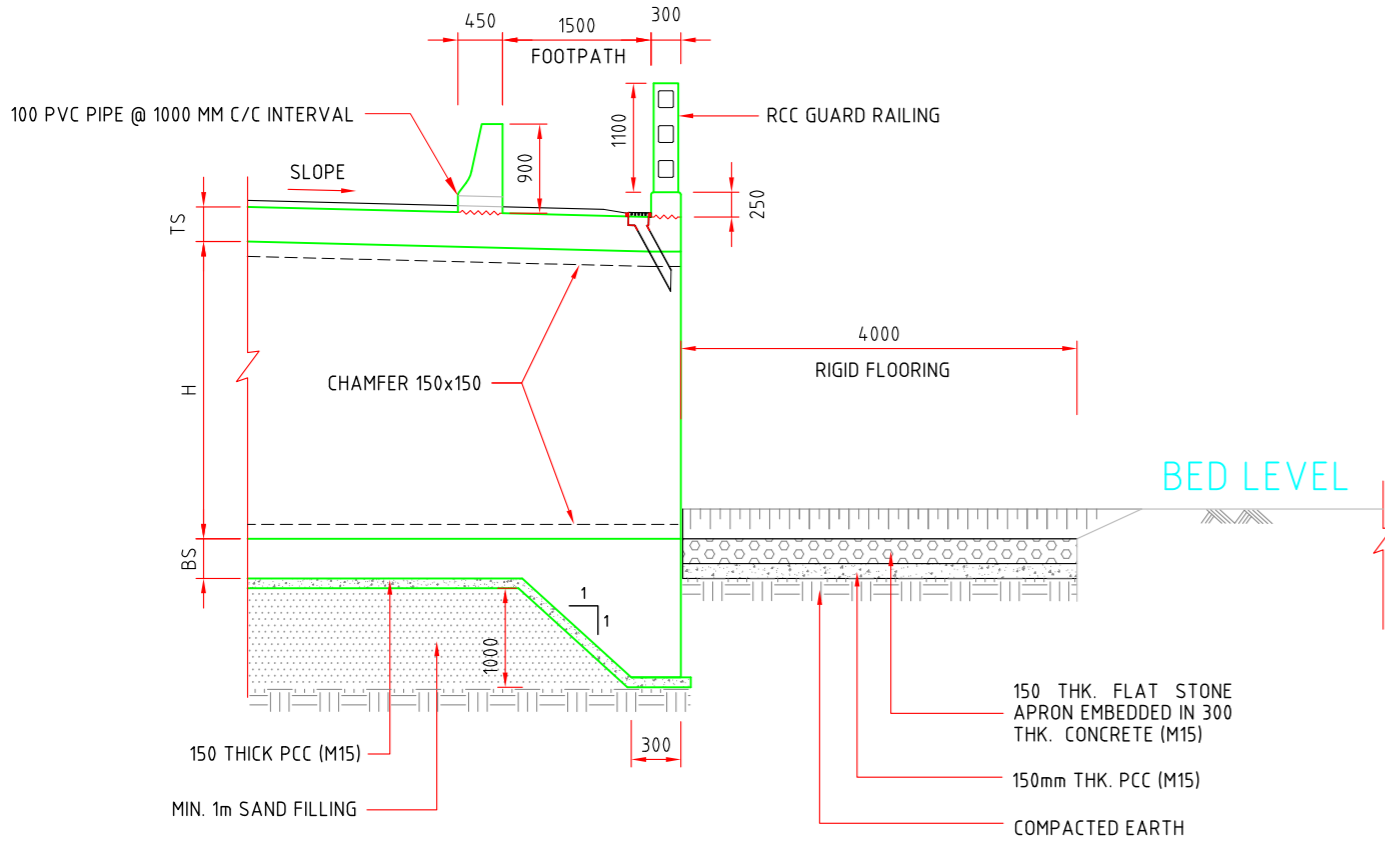


DETAIL-C  
(SCALE 1:30)

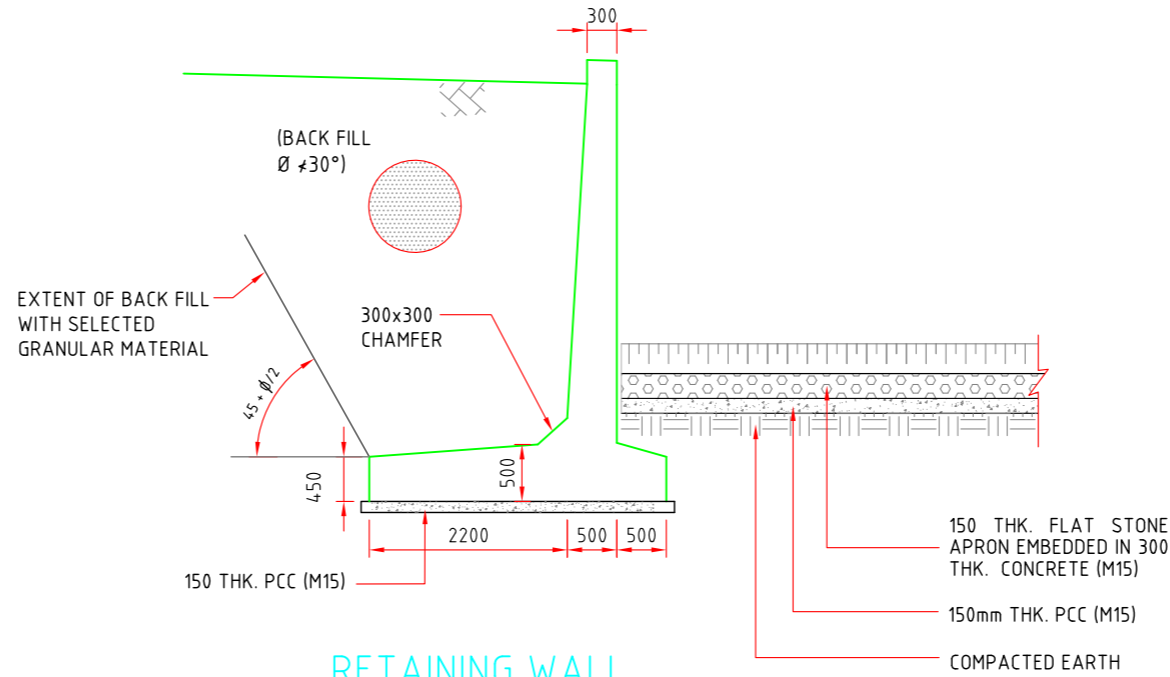
TABLE 1: DIMENSIONS FOR 3-VENT CULVERT

CULVERT SIZE	W	H	EW	MW	TS	BS	G
3x5000x4000	5000	4000*	550	500	500	550	500

\* FOR DESIGN PURPOSE, MAXIMUM "H" HAS BEEN CONSIDERED AS 4000MM. "H" SHALL BE VERIFIED AT SITE BY ENGINEER-IN-CHARGE AND "H" ABOVE 4000MM SHALL BE REPORTED TO THE DESIGN CONSULTANT.



DETAIL-A  
SCALE 1:75



RETAINING WALL  
SCALE 1:75

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CLIENT :-  
Meerut Development Authority  
(MDLA)

DPR CONSULTANT :-  
RITES LIMITED



PROJECT :-

Consultancy Services for Preparation of Detailed  
Project Report (DPR) and Construction Supervision  
of all Infrastructure works

DRAWING STATUS :-

R-0	JAN 25	Tender Drawings
REV	DATE	DESCRIPTION

APPROVED BY

CHECKED BY

DESIGNED BY

DRAWN BY

TITLE

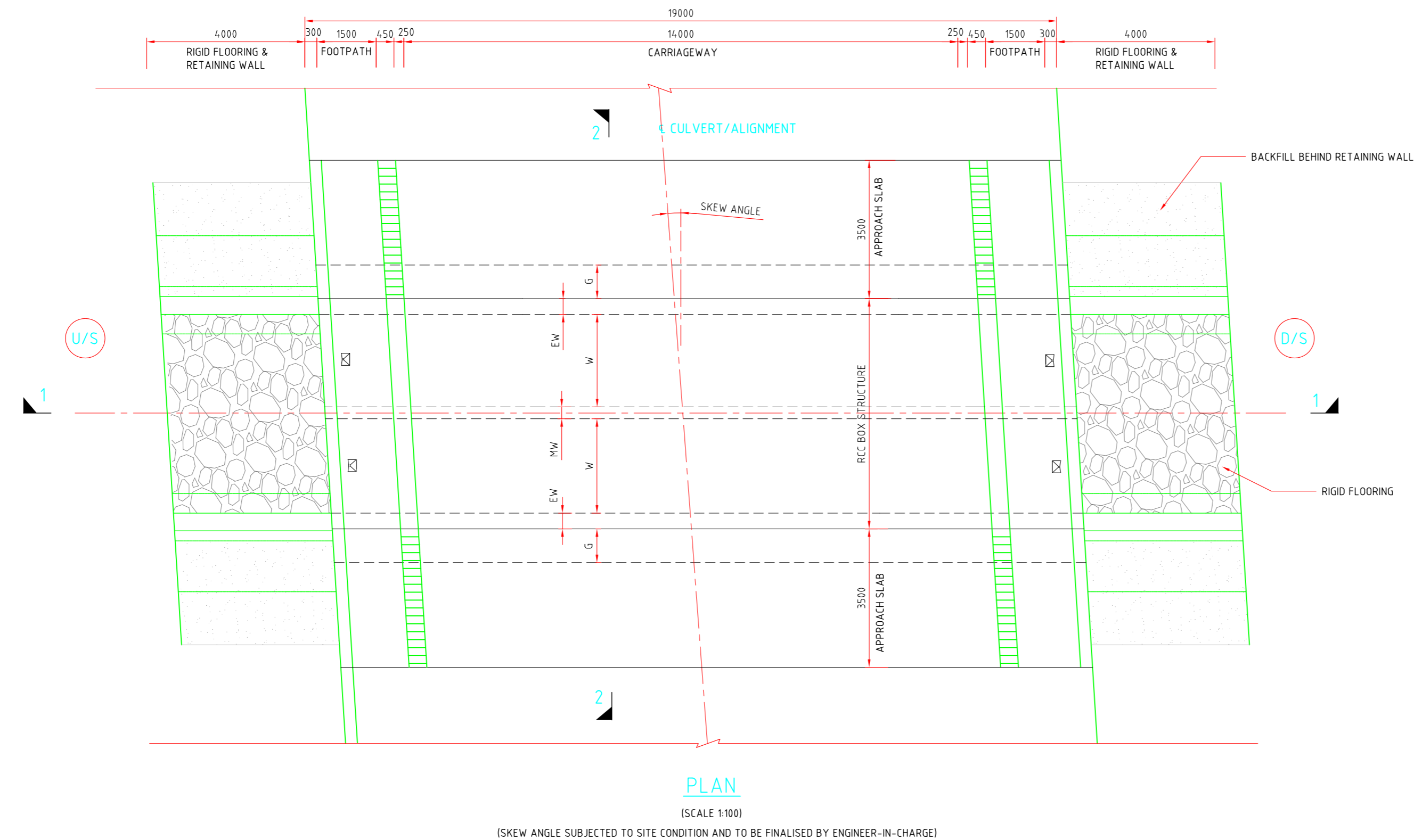
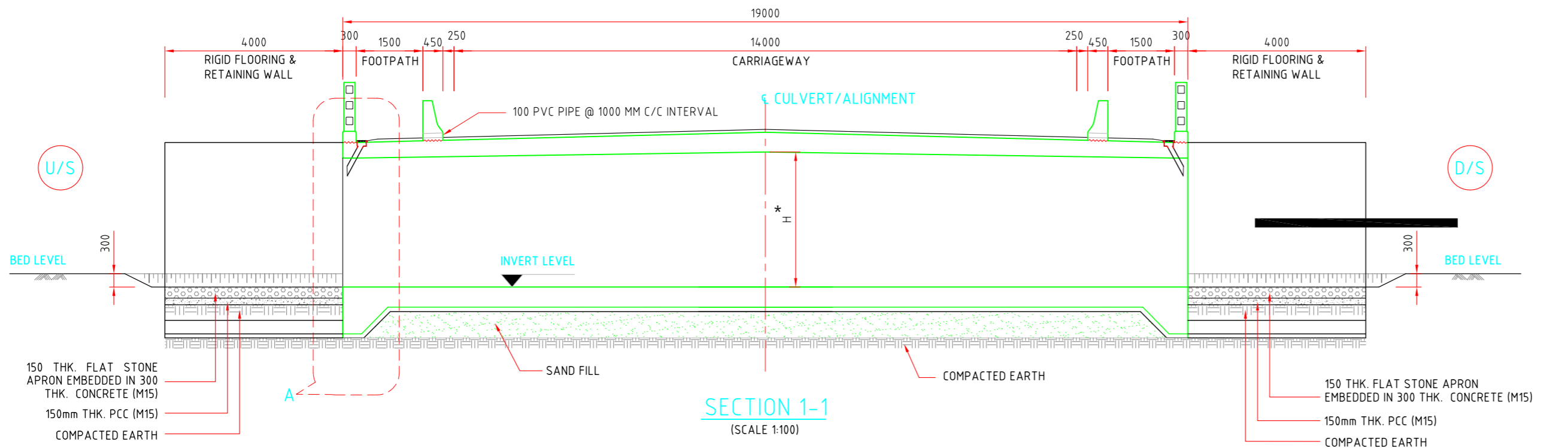
DRAWING NO.

SCALE

Reconstruction of Minor Bridge on Eastern Kutchery  
Road across Abu Nala


AS ABOVE

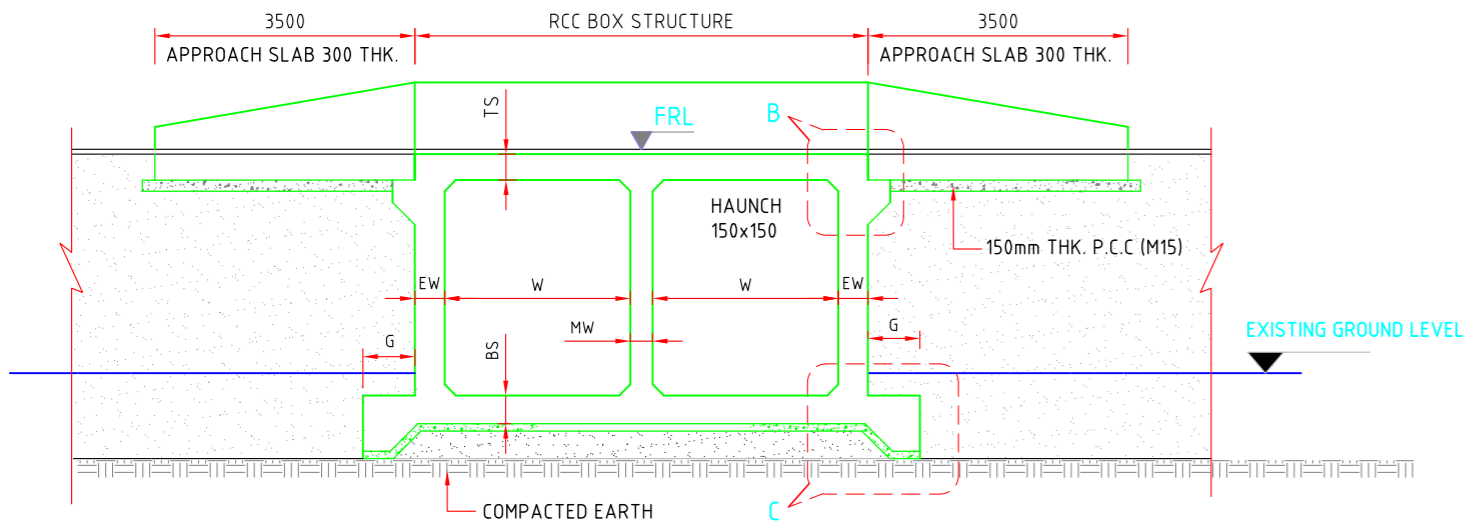
SHEET:- 02 of 02



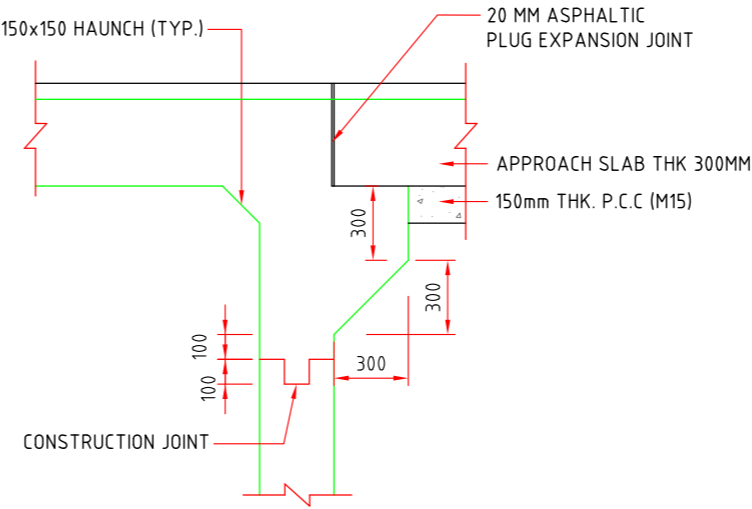
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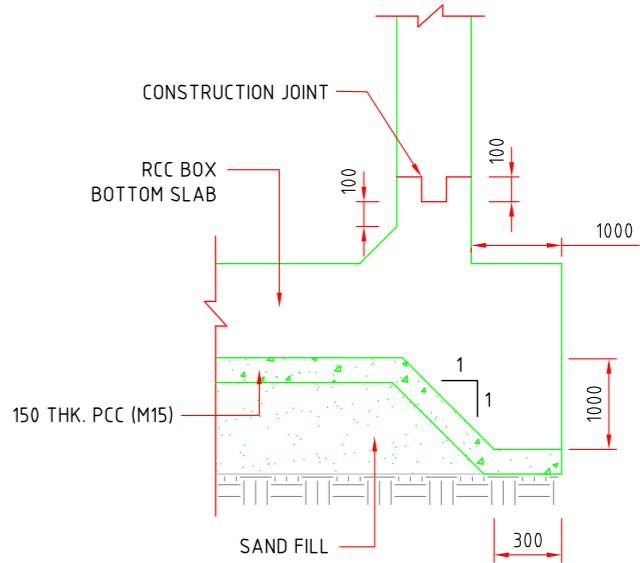
CLIENT :- Meerut Development Authority (MDLA)	DPR CONSULTANT :- RITES LIMITED  	PROJECT :-  Consultancy Services for Preparation of Detailed Project Report (DPR) and Construction Supervision of all Infrastructure works	DRAWING STATUS :-			APPROVED BY			TITLE	Reconstruction of Minor Bridge on Lokpriya Hospital Road across Abu Nala	
						CHECKED BY					
						DESIGNED BY			DRAWING NO.		
			R-0	JAN 25	Tender Drawings						
			REV	DATE	DESCRIPTION	DRAWN BY			SCALE	AS ABOVE	SHEET:- 01 of 02



SECTION 2-2  
SCALE 1:100



DETAIL-B  
(SCALE 1:30)



DETAIL-C  
(SCALE 1:30)

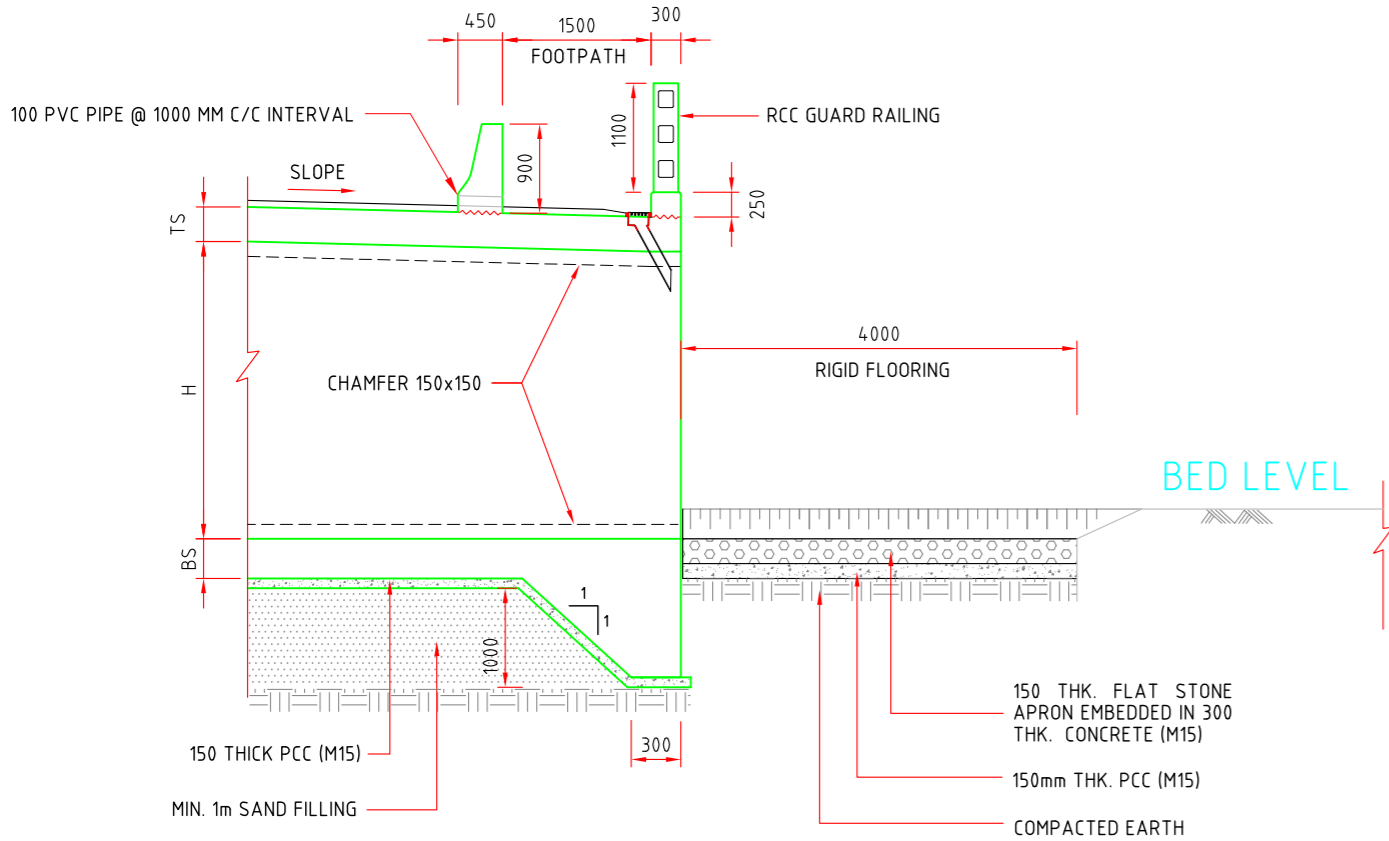
TABLE 1: DIMENSIONS FOR 2-VENT CULVERT

CULVERT SIZE	W	H	EW	MW	TS	BS	G
2x3500x4000	3500	4000*	350	300	350	400	300

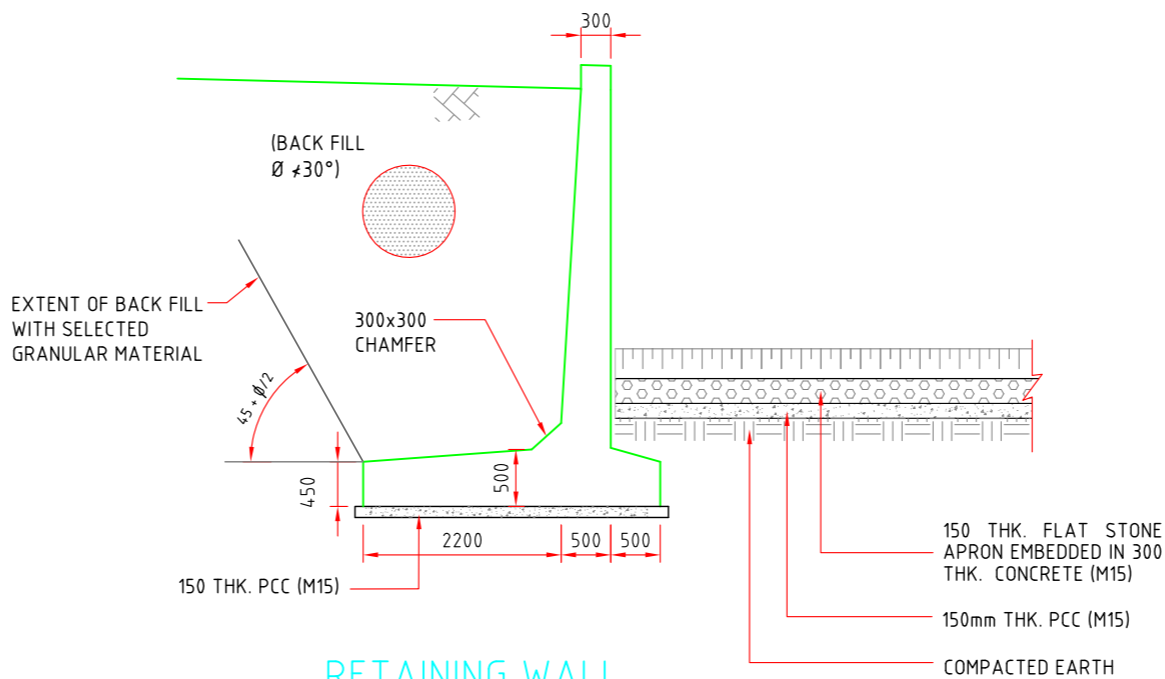
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DETAIL-A  
SCALE 1:75



RETAINING WALL  
SCALE 1:75

CLIENT :-  
Meerut Development Authority  
(MDLA)

DPR CONSULTANT :-  
RITES LIMITED



PROJECT :-

Consultancy Services for Preparation of Detailed  
Project Report (DPR) and Construction Supervision  
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DRAWING STATUS :-

R-0	JAN 25	Tender Drawings
REV	DATE	DESCRIPTION

APPROVED BY

CHECKED BY

DESIGNED BY

DRAWN BY

TITLE

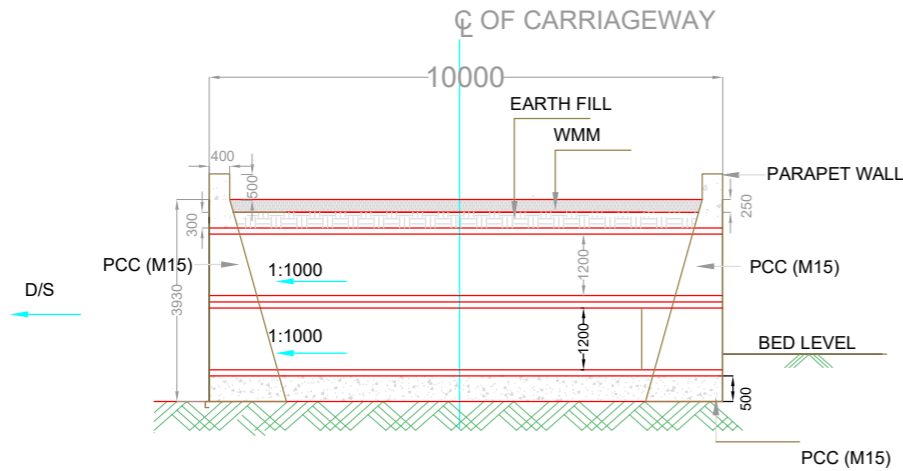
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SCALE

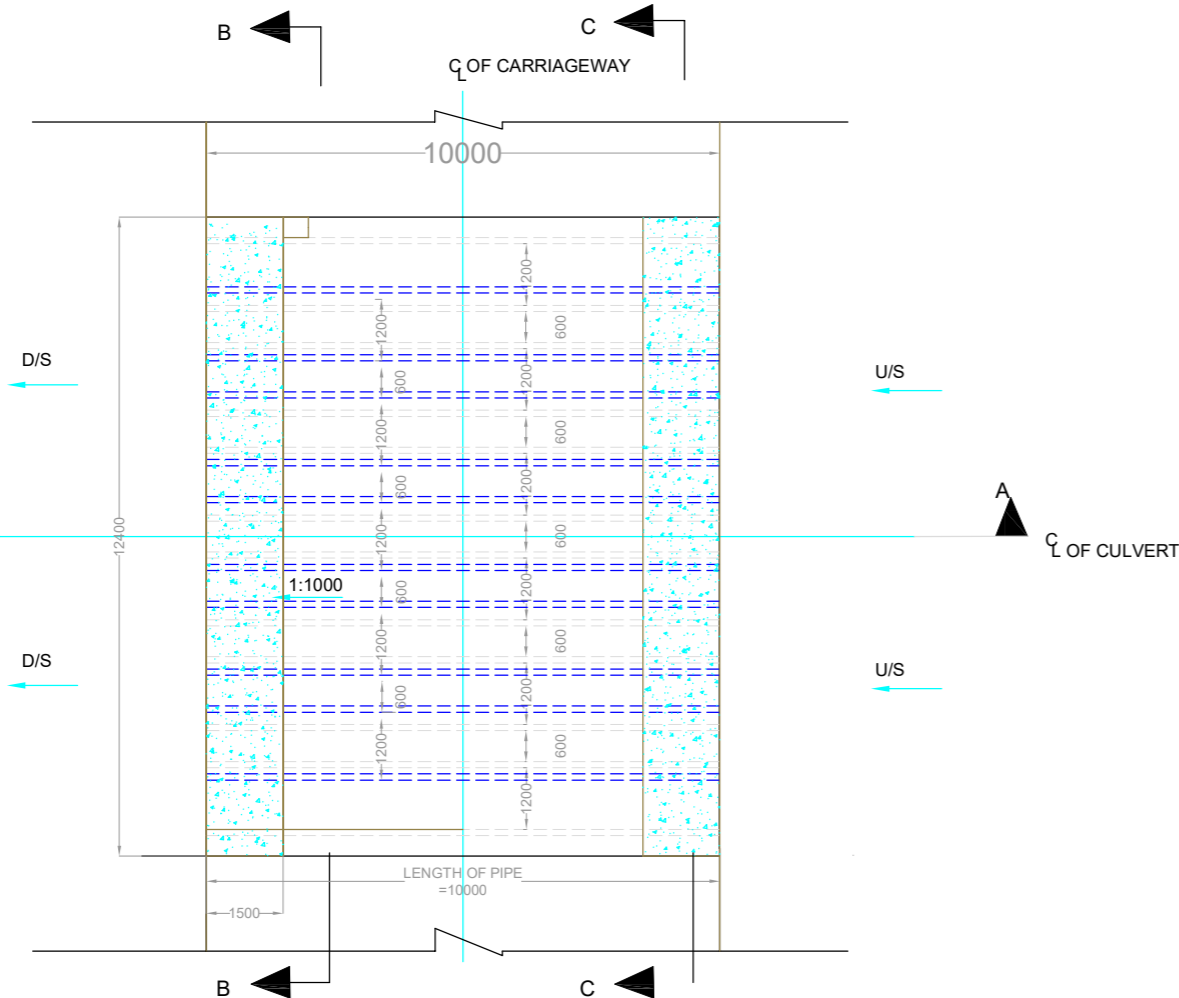
Reconstruction of Minor Bridge on Lokpriya Hospital  
Road across Abu Nala

AS ABOVE

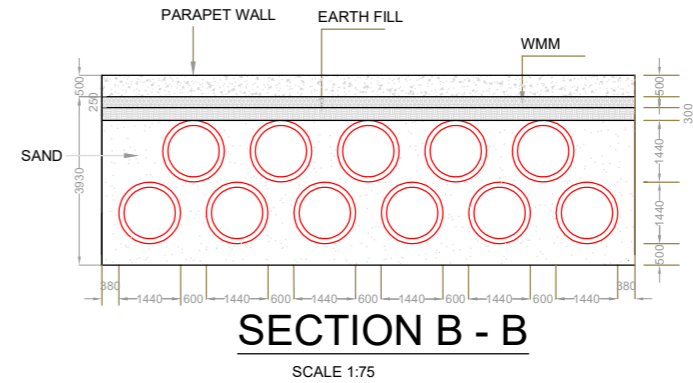
SHEET:- 02 of 02



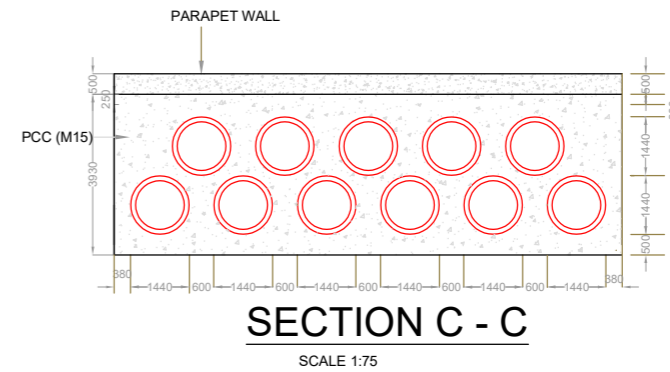
**SECTION AT A-A**  
SCALE 1:100



**PLAN**  
SCALE 1:100



**SECTION B - B**  
SCALE 1:75



**SECTION C - C**  
SCALE 1:75

- NOTES:**
1. ALL DIMENSIONS ARE IN MILLIMETER UNLESS OTHERWISE MENTIONED
  2. NP4 RCC PIPES TO BE USED AS PER IS: 458 - 1988 AND ITS AMMENDMENTS
  3. CRADLE DIMENSIONS AND HEAD WALL LENGTH TO BE ADOPTED AS PER IRC : SP : 13 - 2004
  4. FOR OTHER DETAILS REFER IRC : SP : 13 - 2004
  5. LONGITUDINAL SLOPE OF PIPE SHOULD BE MINIMUM 1 IN 1000
  6. LOCATION OF DIVERSION /CROSSING SHALL BE WITHIN 200M(UPSTREAM) AND 200M (DOWNSTREAM ) FOR LOKPRIYA HOSPITAL MINOR BRIDGE AND EAST KUTCHERY ROAD RESPECTIVELY.
  7. LENGTH AND WIDTH OF DIVERSION BRIDGE MAY BE DECIDED AS PER SITE CONDITION.

CLIENT : -  
Meerut Development Authority  
(MEDA)



NAME OF WORKS:-  
Construction for widening /Reconstruction of Minor Bridges in Meerut in following Road(s) section  
(a) Widening of Minor Bridge on Meerut-Mawana Road(NH34) near Defence Colony.  
(b) Reconstruction of Minor Bridge on Eastern Kutchery Road across Abu Nala.  
(c) Reconstruction of Minor Bridge on Lokpriya Hospital ROAD across Abu Nala